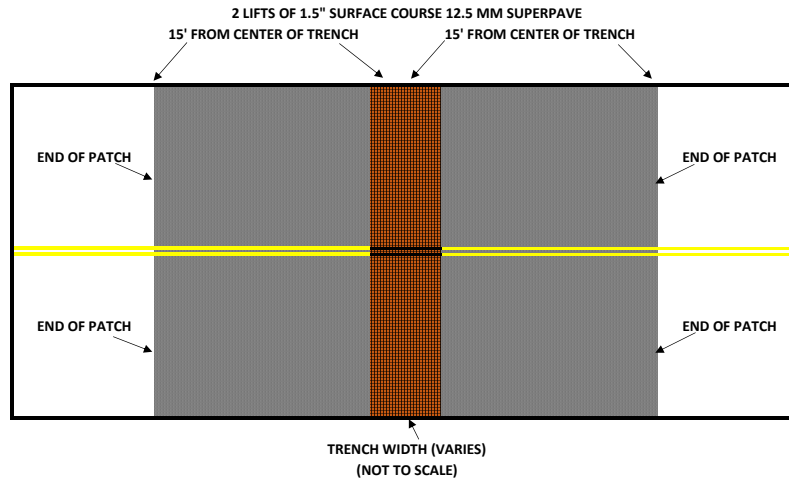
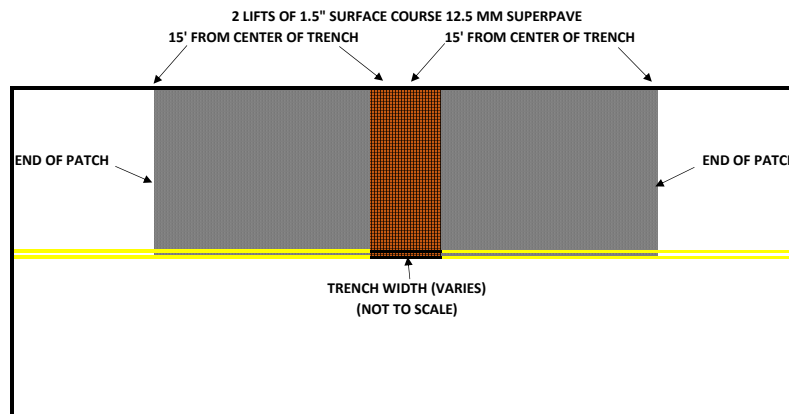


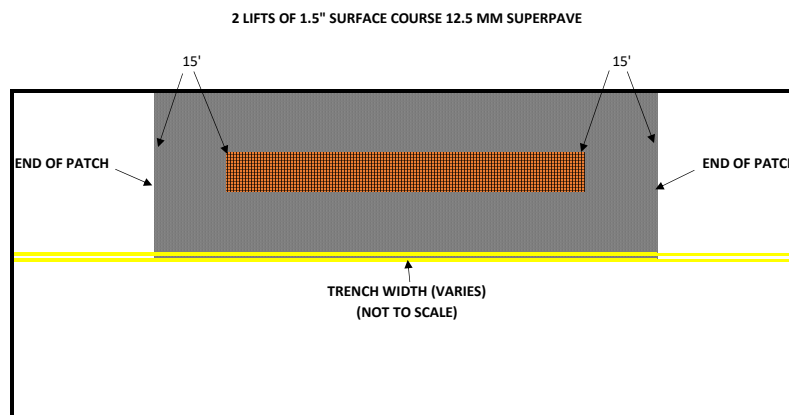
PLAN VIEW OF ROADWAY REPAIR MULTIPLE LANES FULL ROAD WIDTH TO INCLUDE SHOULDERS



PLAN VIEW OF ROADWAY REPAIR ONE LANE TO INCLUDE SHOULDER

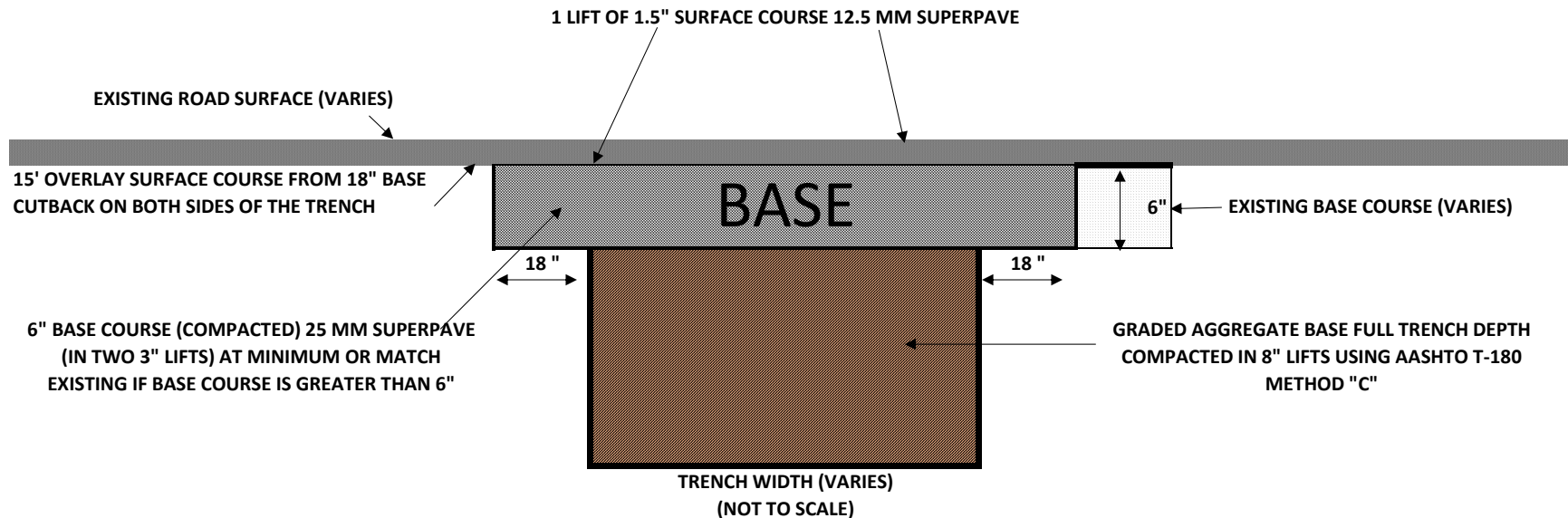


PLAN VIEW OF ROADWAY REPAIR ONE LANE TO INCLUDE SHOULDER



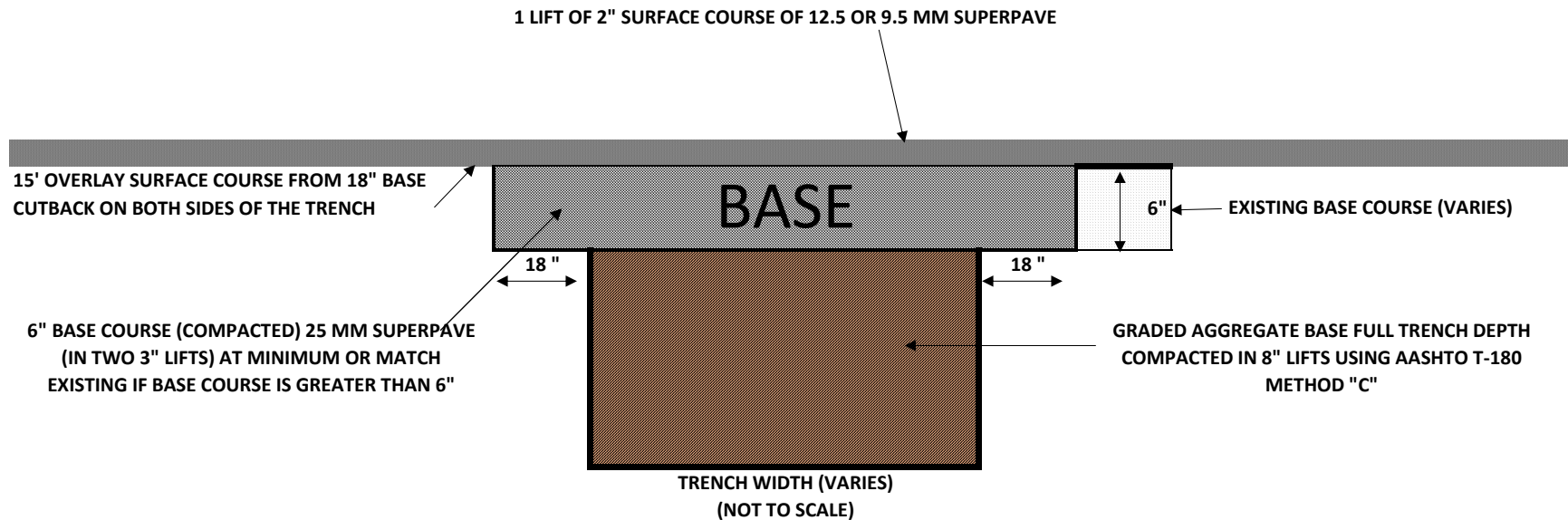
UTILITY TRENCH BACKFILL/PAVEMENT REPAIR DETAIL #1

BITUMINOUS CONCRETE ROADWAYS



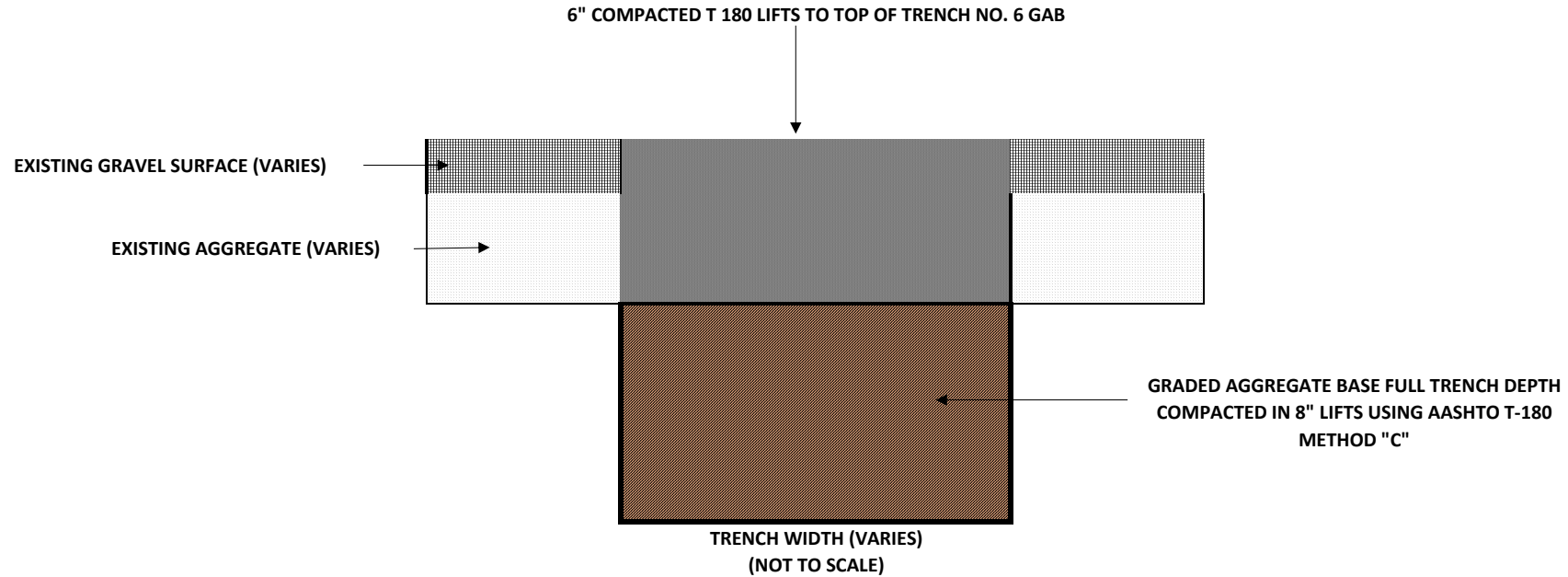
1. BASE COURSE TO EXTEND 18" BEYOND TRENCH CUT ON BOTH SIDES
2. SURFACE COURSE TO EXTEND 15' BEYOND BASE COURSE ON BOTH SIDES
3. BASE COURSE AND AT LEAST ONE SURFACE COURSE LAYER TO BE DONE IN SAME DAY, OR FLUSH WITH EXISTING ROADWAY
4. FINAL SURFACE COURSE LAYER SHALL BE DONE WITHIN 5 CALENDAR DAYS OF THE INITIAL SURFACE COURSE
5. EXISTING PAVEMENT EDGES TO BE SAW CUT
6. ALL VERTICAL EDGES OF THE REPAIR AREA SHALL BE TACKED BEFORE PLACEMENT OF THE SURFACE COURSE
7. THE CONTRACTOR IS REQUIRED TO APPLY FOR AND OBTAIN A "CONSTRUCTION/UTILITY WORK IN COUNTY RIGHT-OF-WAY" PERMIT
8. ENSURE THAT ALL TRENCH BACKFILL, ETC. IS SPECIFIED TO COMPLY WITH THE CONDITIONS OF THIS PERMIT
9. REFER TO "PAVEMENT RESTORATION SPECIFICATIONS & DETAILS" AND "OVERLAY" SPECIFICATIONS FOR MORE DETAILS
10. COMPACTION SHALL BE 92% OF MAXIMUM DENSITY BASED UPON AASHTO T-180 Method "C" EXCEPT FOR THE TOP ONE FOOT (1') OF THE ROADWAY WHICH SHALL BE 97% OF THE MAXIMUM DRY DENSITY BASED UPON AASHTO T-180 Method "C".

PATCHING OF TAR & CHIP ROADWAYS DETAIL #2



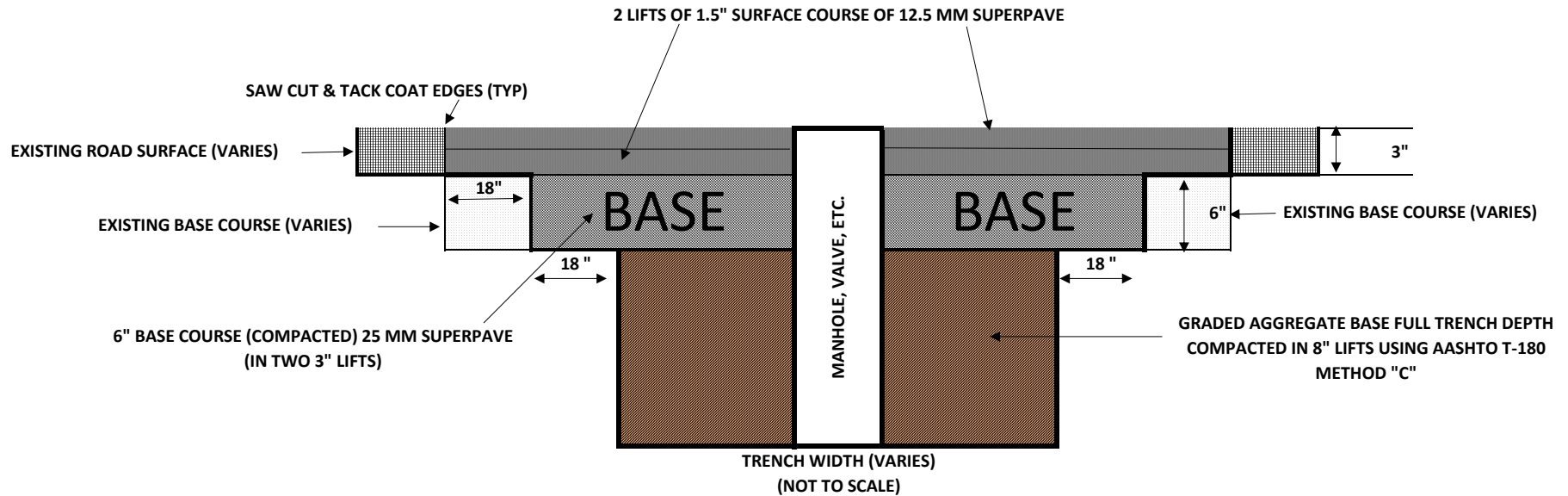
1. BASE COURSE TO EXTEND 18" BEYOND TRENCH CUT ON BOTH SIDES
2. SURFACE COURSE TO EXTEND 15' BEYOND BASE COURSE ON BOTH SIDES
3. BASE COURSE AND SURFACE COURSE LAYER TO BE DONE IN SAME DAY, OR FLUSH WITH EXISTING ROADWAY
4. FINAL SURFACE COURSE LAYER SHALL BE DONE WITHIN 5 CALENDAR DAYS OF THE INITIAL SURFACE COURSE
5. EXISTING PAVEMENT EDGES TO BE SAW CUT
6. ALL VERTICAL EDGES OF THE REPAIR AREA SHALL BE TACKED BEFORE PLACEMENT OF THE SURFACE COURSE
7. THE CONTRACTOR IS REQUIRED TO APPLY FOR AND OBTAIN A "CONSTRUCTION/UTILITY WORK IN COUNTY RIGHT-OF-WAY" PERMIT
8. ENSURE THAT ALL TRENCH BACKFILL, ETC. IS SPECIFIED TO COMPLY WITH THE CONDITIONS OF THIS PERMIT
9. REFER TO "PAVEMENT RESTORATION SPECIFICATIONS & DETAILS" AND "OVERLAY" SPECIFICATIONS FOR MORE DETAILS
10. COMPACTION SHALL BE 92% OF MAXIMUM DENSITY BASED UPON AASHTO T-180 Method "C" EXCEPT FOR THE TOP ONE FOOT (1') OF THE ROADWAY WHICH SHALL BE 97% OF THE MAXIMUM DRY DENSITY BASED UPON AASHTO T-180 Method "C".

PATCHING OF GRAVEL ROADWAYS DETAIL #3



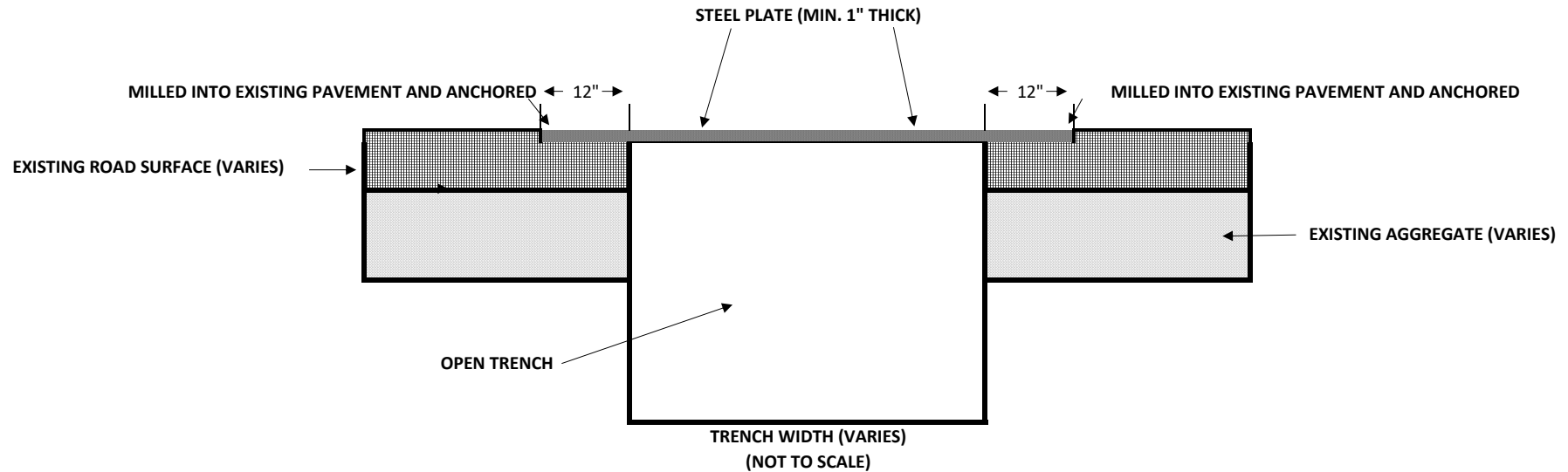
1. TRENCH SHALL BE COMPACTED USING T 180 METHOD IN 6" LIFTS TO 92% COMPACTION, EXCEPT FOR TOP 12" WHICH WILL BE AT 97% COMPACTION
2. THE CONTRACTOR IS REQUIRED TO APPLY FOR AND OBTAIN A "CONSTRUCTION/UTILITY WORK IN COUNTY RIGHT-OF-WAY" PERMIT
3. ENSURE THAT ALL TRENCH BACKFILL, ETC. IS SPECIFIED TO COMPLY WITH THE CONDITIONS OF THIS PERMIT
4. COMPACTION SHALL BE 92% OF MAXIMUM DENSITY BASED UPON AASHTO T-180 Method "C" EXCEPT FOR THE TOP ONE FOOT (1') OF THE ROADWAY WHICH SHALL BE 97% OF THE MAXIMUM DRY DENSITY BASED UPON AASHTO T-180 Method "C".

MANHOLES, WATER METERS & OTHER APPURTENANCES DETAIL #4



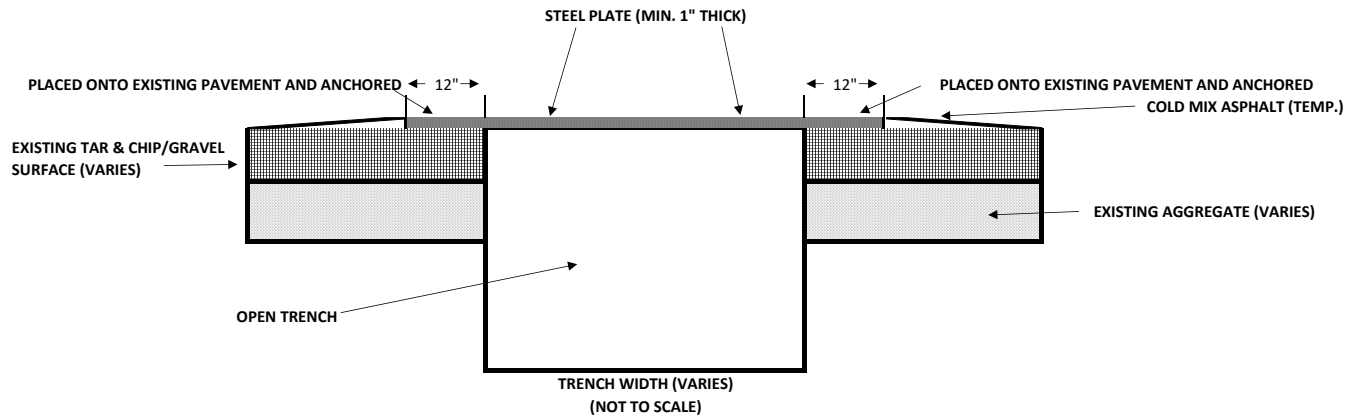
1. BASE COURSE TO EXTEND 18" BEYOND TRENCH CUT ON BOTH SIDES
2. SURFACE COURSE TO EXTEND 18" BEYOND BASE COURSE ON BOTH SIDES
3. BASE COURSE AND AT LEAST ONE SURFACE COURSE LAYER TO BE DONE IN SAME DAY, OR FLUSH WITH EXISTING ROADWAY
4. FINAL SURFACE COURSE LAYER SHALL BE DONE WITHIN 5 CALENDAR DAYS OF THE INITIAL SURFACE COURSE
5. EXISTING PAVEMENT EDGES TO BE SAW CUT
6. ALL VERTICAL EDGES OF THE REPAIR AREA SHALL BE TACKED BEFORE PLACEMENT OF THE SURFACE COURSE
7. THE CONTRACTOR IS REQUIRED TO APPLY FOR AND OBTAIN A "CONSTRUCTION/UTILITY WORK IN COUNTY RIGHT-OF-WAY" PERMIT
8. ENSURE THAT ALL TRENCH BACKFILL, ETC. IS SPECIFIED TO COMPLY WITH THE CONDITIONS OF THIS PERMIT
9. REFER TO "MANHOLES, WATER CAPS" SPECIFICATIONS FOR MORE DETAILS
10. COMPACTION SHALL BE 92% OF MAXIMUM DENSITY BASED UPON AASHTO T-180 Method "C" EXCEPT FOR THE TOP ONE FOOT (1') OF THE ROADWAY WHICH SHALL BE 97% OF THE MAXIMUM DRY DENSITY BASED UPON AASHTO T-180 Method "C".

STEEL PLATING FOR ASPHALT ROADWAYS DETAIL #5



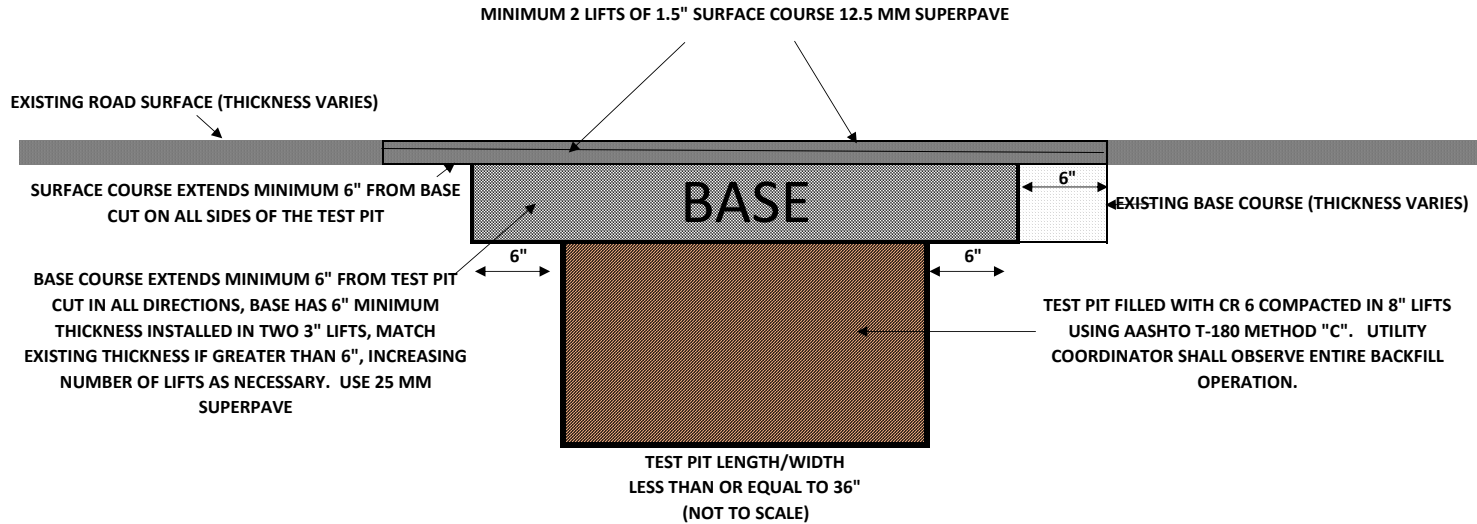
1. EXISTING PAVEMENT EDGES TO BE SAW CUT 12" BEYOND TRENCH WIDTH
2. STEEL PLATES TO BE INSTALLED FLUSH WITH EXISTING PAVEMENT AND ANCHORED
3. THE CONTRACTOR IS REQUIRED TO APPLY FOR AND OBTAIN A "CONSTRUCTION/UTILITY WORK IN COUNTY RIGHT-OF-WAY" PERMIT
4. ENSURE THAT ALL TRENCH BACKFILL, ETC. IS SPECIFIED TO COMPLY WITH THE CONDITIONS OF THIS PERMIT
5. STEEL PLATE SHALL NOT BE USED FOR MORE THAN 48 HOURS WITHOUT SPECIAL PERMISSION FROM THE UTILITY COORDINATOR.
6. REFER TO "STEEL PLATES" SPECIFICATIONS

STEEL PLATING FOR TAR & CHIP AND GRAVEL ROADWAYS DETAIL #6



1. STEEL PLATES TO BE INSTALLED 12" BEYOND EXISTING PAVEMENT AND ANCHORED AND COLD MIX ASPHALT AT EDGES OF STEEL PLATE
2. THE CONTRACTOR IS REQUIRED TO APPLY FOR AND OBTAIN A "CONSTRUCTION/UTILITY WORK IN COUNTY RIGHT-OF-WAY" PERMIT
3. ENSURE THAT ALL TRENCH BACKFILL, ETC. IS SPECIFIED TO COMPLY WITH THE CONDITIONS OF THIS PERMIT
4. STEEL PLATE SHALL NOT BE USED FOR MORE THAN 48 HOURS WITHOUT SPECIAL PERMISSION FROM THE UTILITY COORDINATOR.
5. REFER TO "STEEL PLATES" SPECIFICATIONS

TEST PIT BACKFILL/PAVEMENT REPAIR DETAIL #7 BITUMINOUS CONCRETE ROADWAYS



1. THE CONTRACTOR IS REQUIRED TO APPLY FOR AND OBTAIN A "CONSTRUCTION/UTILITY WORK IN COUNTY RIGHT-OF-WAY" PERMIT PRIOR TO BEGINNING OF WORK.
2. SUB-BASE TEST PIT BACKFILL SHALL BE CR 6, INSTALLED IN 8" LIFTS, AND COMPACTED. NO REPAIR WILL BE ACCEPTED UNLESS TEST PIT BACKFILL OPERATION IS CONDUCTED UNDER UTILITY COORDINATOR OBSERVATION (OR HIS DESIGNEE).
3. ASPHALT BASE COURSE TO EXTEND AT LEAST 6" BEYOND TEST PIT CUT ON ALL SIDES. MINIMUM REPAIR THICKNESS IS 6". REPAIR WILL MATCH EXISTING BASE IF GREATER THAN 6", TO BE INSTALLED IN 3" MAXIMUM THICKNESS LIFTS. USE 25 MM SUPERPAVE.
4. SURFACE ASPHALT COURSE TO EXTEND AT LEAST 6" BEYOND BASE COURSE ON ALL SIDES. MINIMUM REPAIR THICKNESS IS 3". REPAIR WILL MATCH EXISTING SURFACE IF GREATER THAN 3". TO BE INSTALLED IN 1.5" MAXIMUM THICKNESS LIFTS. USE 12.5 MM SUPERPAVE.
5. BASE COURSE AND SURFACE COURSE LAYER REPAIR TO BE DONE IN SAME DAY.
6. EXISTING PAVEMENT EDGES TO BE SAW CUT.
7. ALL VERTICAL EDGES OF THE ASPHALT REPAIR AREA SHALL BE TACKED BEFORE PLACEMENT OF THE SURFACE COURSE.
8. IF TEST PIT IS LESS THAN 24" FROM EDGE OF PAVEMENT OR CENTER SEAM, THEN ASPHALT REPAIR MUST EXTEND TO EDGE OF PAVEMENT OR CENTER SEAM. EDGE OF ASPHALT REPAIR WILL NOT BE ALLOWED WITHIN WHEEL PATH. EXTEND ASPHALT REPAIR BEYOND WHEELPATH AS NECESSARY, TO BE DETERMINED BY THE UTILITY COORDINATOR (OR HIS DESIGNEE).
9. ENSURE THAT ALL TRENCH BACKFILL, ETC. IS SPECIFIED TO COMPLY WITH THE CONDITIONS OF THIS PERMIT.
10. REFER TO "PAVEMENT RESTORATION SPECIFICATIONS & DETAILS" FOR MORE DETAILS.
11. COMPACTION SHALL BE 92% OF MAXIMUM DENSITY BASED UPON AASHTO T-180 Method "C" EXCEPT FOR THE TOP ONE FOOT (1') OF THE ROADWAY WHICH SHALL BE 97% OF THE MAXIMUM DRY DENSITY BASED UPON AASHTO T-180 Method "C".
12. IF TEST PIT IS GREATER THAN 36" LONG OR WIDE USE MANHOLES, WATER METERS & OTHER APPURTENANCES DETAIL #4.